



July 31, 2012

Mr. Paul Glaab, Chairman, Board of Directors
Orange County Transportation Authority
P.O. Box 14184
Orange, CA 92863-1584

Subject: I-405 Improvement Project

Dear Chairman Glaab:

The I-405 Corridor Cities of Costa Mesa, Fountain Valley, Huntington Beach, Los Alamitos, Seal Beach, and Westminster (Cities) collectively appreciate Orange County Transportation Authority's (OCTA) efforts in coordinating transportation improvements throughout Orange County and implementing transportation solutions that are vital to the long-term needs of residents and visitors to the Orange County area.

The Cities have carefully considered the alternatives for the I-405 Freeway expansion in the Draft Environmental Impact Report/Environmental Impact Statement (EIR/EIS), and are unified in our opposition to Alternative 3 with Express Lanes. The Mayors of the Cities do, however; support Alternative 2 as our "locally preferred alternative," provided certain conditions are included to address the concerns of each City as outlined in Attachment 1. Alternative 2 provides the greatest travel time savings and vehicle capacity in the general purpose lanes and does not convert existing lanes to toll lanes nor require the paying of tolls.

The following are the bases for this determination:

- The Renewed Measure M program approved by the voters of Orange County did not include the Express Lanes solution for the I-405 Freeway and did not authorize the use for M2 funds to facilitate the implementation of Express Lanes;
- Alternative 3 results in the loss of the "free" High Occupancy Vehicle (HOV) lane that is replaced by "toll" Express Lanes;
- Alternative 3 burdens a large segment of the population to obtain transponders and pay to use the tax-payer funded improvements;
- The number of lanes that are available "free" for the public for most of the stretch of the freeway for Alternative 3 remains the same as currently exists, at five lanes total, whereas Alternative 2 proposes seven (Free) lanes;
- Alternative 2, at a cost of approximately \$300 million less than Alternative 3, provides a better value for the money in terms of benefits to the communities and commuters;
- Alternative 2 has less right-of-way impact than Alternative 3; and
- Alternative 2 represents a better utilization of land and money resources.

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The Mayors of the Cities jointly request OCTA to champion the cause of the local cities by finding that Alternative 2 is the only "locally preferred alternative" for the I-405 Improvement Project.

Sincerely,



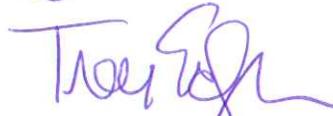
Eric R. Bever
Mayor, Costa Mesa



John Collins
Mayor, Fountain Valley



Devin Dwyer
Mayor Pro Tem, Huntington Beach



Troy D. Edgar
Mayor, Los Alamitos



Michael Levitt
Mayor, Seal Beach



Margie L. Rice
Mayor, Westminster

Attachment 1 – Suggested Revisions to Alternative 2

- c Senator Tom Harman
- Assembly Member Allan R. Mansoor
- Assembly Member Jim Silva
- Costa Mesa City Council
- Fountain Valley City Council
- Huntington Beach City Council
- Los Alamitos City Council
- Seal Beach City Council
- Westminster City Council
- James Pinheiro, Caltrans, Deputy District Director
- Staff

SUGGESTED REVISIONS TO ALTERNATIVE 2City of Costa Mesa

- There needs to be close coordination with local agencies on all ramp closures and to address construction-related impacts. Local events and conditions should be taken into account in scheduling closures.
- There should be full consideration to interior noise levels of sensitive uses and appropriate abatement of any noise impacts to achieve minimally acceptable interior noise standards.
- The proposed direct connector from eastbound Ellis Avenue to southbound I-405 Freeway requires widening of the freeway adjacent to Moon Park and several residences along Nevada Avenue in Costa Mesa. The proposed project will cause a retaining wall of up to 16 feet and a sound wall on top of it of 12 feet in height to be constructed approximately 6 feet from the backyard of residences. This will create significant noise, air quality and visual impacts. Several solutions should be considered to address this including deviating from State lane and shoulder widths standards, planting of mature trees and high density vegetation between the retaining wall and residences and also relinquishing an excess portion of right-of-way to adjacent residences.

City of Fountain Valley

Please refer to the comments to the Draft Environmental Impact Report/Environmental Impact Statement (EIR/EIS) submitted by the City of Fountain Valley.

City of Huntington Beach

Please refer to the comments to the Draft Environmental Impact Report/Environmental Impact Statement (EIR/EIS) submitted by the City of Huntington Beach.

City of Los Alamitos

Please refer to the comments to the Draft Environmental Impact Report/Environmental Impact Statement (EIR/EIS) submitted by the City of Los Alamitos.

City of Seal Beach

- Retention of the current soundwall on the north side of the I-405 freeway between Valley View Street and Seal Beach Boulevard by modification of the project to match City of Seal Beach modified design for Alternative 2; and
- Construct the improvements in a phased approach. The first phase will be to add only one northbound lane now between Valley View Street and the I-605 Freeway. The second phase will add a second lane in conjunction with I-405 improvements north of the county line.

City of Westminster

- Provide roadway alignment design alternatives that promote a more balanced approach to right-of-way acquisition.
- Mitigate grade differentials between street elevation and private property elevation resulting from the proposed bridge reconstructions.
- Mitigate on-site circulation and parking impacts for improved site access management.
- Vacate surplus property, not used for the project and deemed unusable by the State, to the City to enhance areas impacted by the project.
- Assess the potential for congestion relief on the local circulation system; provide design alternatives that improve adjacent local roads/connectors and promote ADA compliant pedestrian facilities.
- Provide a more balanced and fair approach to sound wall installation and replacement; beyond what is currently presented in the Draft EIS/EIR.